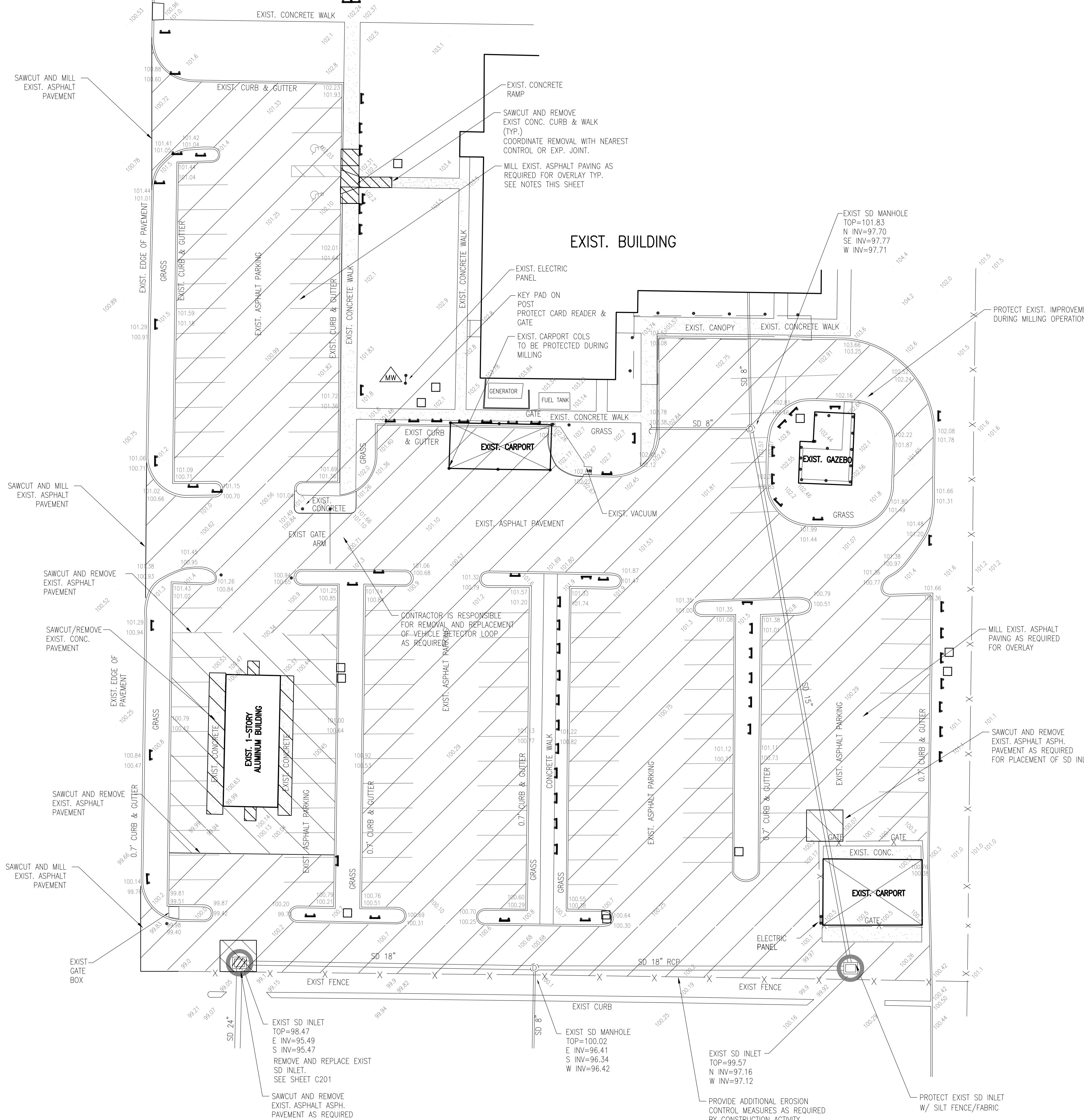
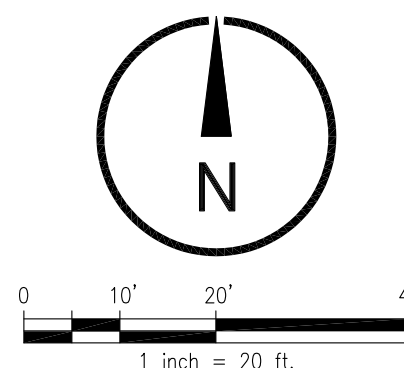


33RD STREET



DEMOLITION AND EROSION CONTROL PLAN

SCALE: 1" = 20'

SURVEYOR'S REPORT:

- 1. UTILITY LOCATIONS SHOWN HEREON ARE BASED ON FIELD LOCATIONS OF MARKINGS BY SOUTHEASTERN SURVEYING & MAPPING CORP. FIELD MARKINGS ARE BASED ON SIGNALS RECEIVED FROM GROUND PENETRATING RADAR (GPR) AND ELECTRONIC EQUIPMENT. LOCATIONS ARE APPROXIMATE AND TEST HOLES SHOULD BE PERFORMED FOR VERIFICATION.
2. VERTICAL INFORMATION SHOWN HEREON REFERS TO ORANGE COUNTY BENCHMARK D-235-005, BEING A 3" BRASS DISK SET IN CONCRETE WITH AN ELEVATION OF 100.732 STAMPED ON DISK BEING IN THE NATIONAL GEODETIC VERTICAL DATUM OF 1929 (NGVD 29).
3. HORIZONTAL FEATURES SHOWN ON THE MAP REFER TO A NATIONAL GEODETIC SURVEY POINT WITH DESIGNATION 'GIS 0131 KAREN MCKEE', PID NUMBER AK7129 AND IS RELATIVE TO NORTH AMERICAN DATUM OF 1983 (NAD83), 2011 ADJUSTMENT, STATE PLANE COORDINATE SYSTEM, FLORIDA EAST ZONE. DISTANCES SHOWN ARE GRID DISTANCES.

SOIL EROSION AND SEDIMENTATION CONTROL NOTES

- 1. ALL EROSION AND SEDIMENT CONTROL WORK SHALL CONFORM TO ORANGE COUNTY AND ST JOHNS RIVER WATER MANAGEMENT DISTRICT (SJRWMD).
2. EROSION AND SEDIMENT CONTROL MEASURES ARE TO BE PLACED PRIOR TO, OR AS THE FIRST STEP IN, CONSTRUCTION.
3. SOIL MATERIALS, SHALL BE CONTAINED, AND NOT ALLOWED TO COLLECT ON ANY OFF-PERIMETER AREAS OR IN WATERWAYS. THESE INCLUDE BOTH NATURAL AND MAN-MADE OPEN DITCHES, STREAMS, STORM DRAINS, LAKES AND POND. DAILY INSPECTIONS SHALL BE MADE BY THE CONTRACTOR TO DETERMINE THE EFFECTIVENESS OF THESE EFFORTS. ANY NECESSARY REMEDIES SHALL BE PERFORMED WITHOUT DELAY.
4. ALL MUD, DIRT OR OTHER MATERIALS TRACKED OR SPILLED ONTO EXISTING STATE-COUNTY-CITY OR PRIVATE ROADS AND FACILITIES FROM THIS SITE, DUE TO CONSTRUCTION SHALL BE PROMPTLY REMOVED BY THE CONTRACTOR/BUILDER.
5. PERMANENT SOIL EROSION CONTROL MEASURES FOR ALL SLOPES, CHANNELS, DITCHES OR ANY DISTURBED LAND AREAS SHALL BE COMPLETED WITHIN (15) FIFTEEN CALENDAR DAYS AFTER FINAL GRADING. WHEN IT IS NOT POSSIBLE TO PERMANENTLY PROTECT A DISTURBED AREA IMMEDIATELY AFTER GRADING OPERATIONS, TEMPORARY EROSION CONTROL MEASURES SHALL BE INSTALLED. ALL TEMPORARY PROTECTION SHALL BE MAINTAINED UNTIL PERMANENT MEASURES ARE IN PLACE AND ESTABLISHED. A CERTIFICATE OF COMPLIANCE WILL NOT BE ISSUED UNTIL THE ABOVE REQUIREMENTS HAVE BEEN MET.
6. EXISTING DRAINAGE SYSTEM AND INLETS SHALL BE PROTECTED BY USING FILTER FABRIC CLOTHED AROUND INLETS AND BENEATH GRATES.

SITE DEMOLITION NOTES

- 1. ALL DEMOLITION AND CONSTRUCTION ACTIVITY WILL BE ACCOMPLISHED IN APPROPRIATE PHASES IN ORDER TO KEEP EXISTING FACILITY OPERATIONAL.
2. DISPOSAL OF ALL MATERIAL LEAVING THE SITE WILL BE ON HAUL ROUTES, AND BY METHODS AS APPROVED BY OWNER.
3. COORDINATE LOCATION OF CONTRACTOR STAGING AREA WITH OWNER CONSTRUCTION REPRESENTATIVE.
4. ALL PROPERTY AFFECTED BY THIS WORK SHALL BE RESTORED TO A CONDITION EQUAL TO OR BETTER THAN EXISTED UNLESS SPECIFICALLY EXEMPTED BY THE PLANS. THE COST FOR SUCH RESTORATION SHALL BE INCIDENTAL TO OTHER CONSTRUCTION AND NO EXTRA COMPENSATION WILL BE ALLOWED.
5. ALL SURVEY DATA USED AND CONDITIONS ASSUMED TO BE PRESENT IN PREPARATION OF THESE PLANS WAS PROVIDED BY SOUTHEASTERN SURVEYING & MAPPING, INC.
6. CONTRACTOR SHALL CLEAN THE CONSTRUCTION AREA OF ALL DEBRIS, AGGREGATES, ETC. ALL WASTE MATERIAL SHALL BE REMOVED FROM SITE AND DISPOSED OF IN A LEGAL MANNER.
7. CONTRACTOR SHALL PROTECT THE EDGE OF ALL REMAINING PAVEMENT AND HARDSCAPE. DO NOT HAUL OVER UNPROTECTED PAVEMENT OR SAWCUT EDGES.
8. CONTRACTOR SHALL VERIFY LOCATION OF ALL UNDERGROUND UTILITIES AND IMPROVEMENTS PRIOR TO START OF CONSTRUCTION.
9. CONTRACTOR WILL BE RESPONSIBLE FOR REMOVAL AND REPLACEMENT OF EXISTING SITE IMPROVEMENTS AS REQUIRED FOR MILLING OPERATION. THIS MAY INCLUDE FENCING, PAVEMENT MARKERS, WHEEL STOPS, ETC.
10. NOISE LEVELS AND HOURS OF DEMOLITION OPERATIONS SHALL BE AS DIRECTED BY OWNER.
11. CONTRACTOR SHALL RECORD CONFIGURATION OF EXISTING STRIPING ON SITE AND BE RESPONSIBLE FOR LAYOUT AND RESTRIPING UPON COMPLETION OF PROJECT.
12. REMOVE AND REPLACE ANY DAMAGED CONCRETE CURB AS NOTED WITHIN PROJECT AREA.
13. EXISTING SITE MAY CONTAIN ABOVE AND BELOW GROUND IMPROVEMENTS NOT DEPICTED ON THIS PLAN. CONTRACTOR SHALL VERIFY SITE CONDITIONS WITH OWNER'S REPRESENTATIVE AND BE RESPONSIBLE FOR REMOVAL OR RELOCATION OF EXISTING IMPROVEMENTS INTERFERING WITH NEW DEVELOPMENT.

PAVING NOTES:

- 1. CONTRACTOR SHALL MILL EXISTING PAVEMENT AS REQUIRED FOR PLACEMENT OF NEW WEARING COARSE. MILLING AND PAVING OPERATION SHALL BE CONDUCTED IN APPROPRIATE PHASES IN ORDER TO UTILIZE THE EXISTING PARKING AREA BY THE USER.
2. PROTECT EXISTING IMPROVEMENTS DURING MILLING OPERATION. MINIMUM MILL DEPTH IS 1.5". HOWEVER THE DEPTH SHALL BE INCREASED WHERE REQUIRED FOR REPAIR TO EXISTING BASE, PAVING, AND ELIMINATION OF DRAINAGE BIRD BATHS.
3. IF EXISTING BASE IS EXPOSED DURING MILLING OPERATION, CONTRACTOR SHALL PROTECT EXISTING BASE BY APPLICATION OF PRIME COAT TO EXPOSED BASE, WITHIN THE SAME DAY AS THE EXPOSURE.
4. ALL MILLING AND RESURFACING OPERATION SHALL COMPLY WITH LATEST FDOT STANDARD SPECIFICATIONS FOR ROADWAY AND BRIDGE CONSTRUCTION.
5. CONTRACTOR HAS THE OPTION OF REMOVING EXISTING ASPHALT PAVEMENT BY MECHANICAL MEANS IN ITS ENTIRETY, HOWEVER ANY COSTS ASSOCIATED WITH REMOVAL OF ENTIRE SURFACE COARSE, REPAIR OF DAMAGED BASE, AND NECESSARY WORK TO MATCH EXISTING GRADES AT SITE IMPROVEMENTS SHALL BE THE RESPONSIBILITY OF CONTRACTOR.

PHASING NOTES

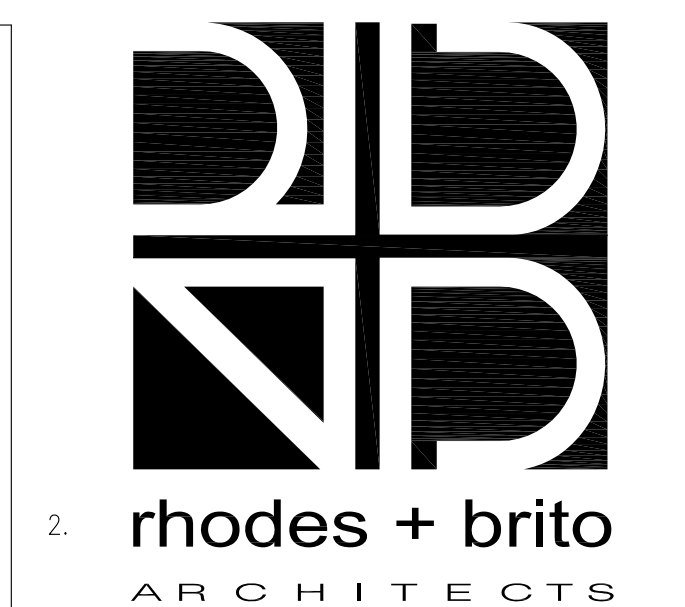
- 1. CONTRACTOR IS RESPONSIBLE FOR MAINTENANCE FOR VEHICLE AND PEDESTRIAN TRAFFIC ON-SITE DURING CONSTRUCTION. CONTRACTOR SHALL MAINTAIN VEHICULAR AND PEDESTRIAN ACCESS BY USE OF BARRICADES, TEMPORARY WALKS, FENCING, ETC.
2. CONTRACTOR SHALL COORDINATE CONSTRUCTION PHASING WITH OWNER'S REPRESENTATIVE. CONSTRUCTION PHASING SHALL BE CONDUCTED IN A MANNER AS ACCEPTABLE TO OCCP.
3. CONTRACTOR SHALL MAINTAIN UTILITY SERVICE TO EXISTING BUILDING DURING ENTIRE CONSTRUCTION PERIOD. CONTRACTOR SHALL PROVIDE ALL TEMPORARY MEASURES AS REQUIRED.
4. ACCESS TO EXIST. FACILITY SHALL BE MAINTAINED DURING ENTIRE CONSTRUCTION PERIOD.

PROJECT SCOPE

PROJECT SCOPE CONSISTS OF MILLING AND RESURFACING OF EXIST. PAVEMENT ONSITE.

LEGEND

Legend table with columns 'EXIST.' and 'TO BE REMOVED'. Includes symbols for ASPHALT PAVEMENT, ASPHALT PAVEMENT (MILLING AREA), CONCRETE CURB, CONCRETE WALK, FENCE, POWER POLE, LIGHT POLE, FIRE HYDRANT, TREE, FINISH GRADE, STORM DRAIN LINE, STORM DRAIN INLET, STORM M.H., SAN. SEWER LINE, SAN. SEWER MH.



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Consultants

OCSO SECTOR IV PARKING LOT DESIGN

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Seal

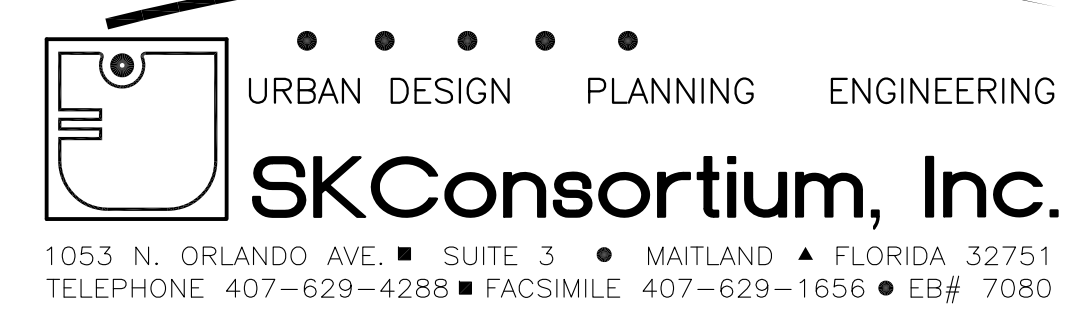
MAJID KALAGHCHI
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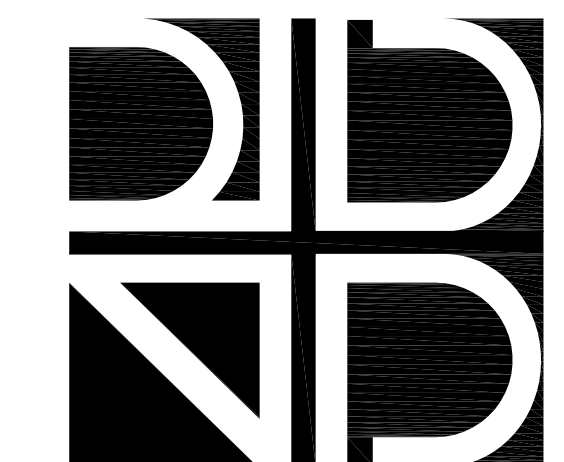
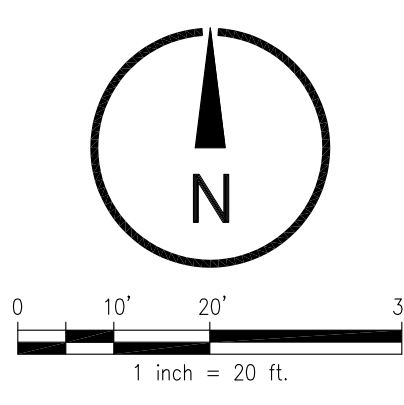
Table with columns: DATE, SUBMISSION / REVISION, NO.

DEMOLITION AND EROSION CONTROL PLAN

SCALE: 1"=20'
DRAWN BY: JF
CHECK BY: MK
DATE: JANUARY 6, 2017
PROJECT NUMBER: 15012-0013



C001



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GEOMETRY NOTES

- ALL SURVEY DATA USED AND CONDITIONS ASSUMED TO BE PRESENT IN PREPARATION OF THESE PLANS WAS PROVIDED BY SOUTHEASTERN SURVEYING & MAPPING.
- THE CONTRACTOR SHALL VERIFY AND LOCATE ALL VERTICAL AND HORIZONTAL CONTROL POINTS PRIOR TO CONSTRUCTION. IF ANY DISCREPANCIES SHOULD BE FOUND, THE CONTRACTOR SHALL NOTIFY THE ENGINEER AND SURVEYOR OF THE CONDITION IN WRITING PRIOR TO COMMENCING HIS CONSTRUCTION ACTIVITIES.
- ALL PAVEMENT OFFSETS, RADII AND DIMENSIONS SHOWN ARE TO PROPOSED EDGE OF PAVEMENT, UNLESS OTHERWISE NOTED.
- THE CONTRACTOR SHALL STAKE ALL IMPROVEMENTS USING THE GEOMETRIC DATA PROVIDED. IT IS THE CONTRACTOR'S SOLE RESPONSIBILITY TO COMPLETELY STAKE AND CHECK ALL IMPROVEMENTS TO ENSURE ADEQUATE POSITIONING, BOTH HORIZONTAL AND VERTICAL, PRIOR TO THE INSTALLATION OF ANY IMPROVEMENTS.

GENERAL NOTES

- TRAFFIC MARKINGS NOTES:
 - ALL DRIVEWAYS EXTING ONTO ANY PUBLIC OR PRIVATE STREET MUST HAVE A TRAFFIC CONTROL DEVICE (DOT STOP SIGN MOUNTED 7' ABOVE THE PEDESTRIAN VEHICLE TRAVEL WAY (MEASURED FROM BOTTOM OF SIGN) AND THROUGHOUT THE SITE AS NECESSARY FOR SAFETY.
 - ALL SIGN INSTALLATIONS SHALL COMPLY WITH SIZE, LOCATION AND HEIGHT (7') AS OUTLINED IN THE "MUTCD".
 - PAVEMENT MARKINGS - GENERAL PRINCIPLES (SECTION 3A-5):
 - PARKING LOT PAVEMENT MARKINGS SHALL BE WHITE PAINTED (DIRECTIONAL ARROWS, STOP BARS, LINES DESIGNATING PARKING SPACES AND HANDICAPPED PARKING SYMBOLS).
 - ALL HANDICAPPED PARKING SPACES MUST BE LEGALLY SIGNED WITH ONE REGULATION HANDICAP SIGN AND ONE WHITE PAVEMENT MARKING SYMBOL PER SPACE.
 - A 24" STOP BAR SHALL BE PROVIDED AT ALL POINTS OF EGRESS IN CONFORMANCE WITH FDOT #17346, DRAWING #3 OF 8.
- HANDICAP RAMP SHALL COMPLY WITH FLORIDA DEPARTMENT OF TRANSPORTATION ROADWAY AND TRAFFIC DESIGN STANDARDS INDEX NO. 0304, AND ORANGE COUNTY REQUIREMENTS WITH TRUNKATED DOME MATS.
- ALL CONSTRUCTION SHALL COMPLY WITH FDOT SPECIFICATIONS FOR ROADWAY AND BRIDGE CONSTRUCTION LATEST EDITION AND STANDARDS OF ORANGE COUNTY.

PAVING NOTES

- CONTRACTOR IS RESPONSIBLE FOR SUBMISSION OF PRODUCT DATA, DESIGN MIX, METHOD OF APPLICATION, ETC. TO OWNER'S REPRESENTATIVE FOR REVIEW AND APPROVAL PRIOR TO START OF CONSTRUCTION.
- COORDINATE LOCATION OF CONTRACTOR STAGING AREA WITH OWNER'S REPRESENTATIVE.
- ALL PAVEMENT MARKINGS SHALL CONFORM TO MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AND FDOT SPECIFICATIONS. ALL NEW PAVEMENT MARKINGS ARE TO BE INSTALLED IN ACCORDANCE WITH FDOT INDEX NO. 710.
- CONTRACTOR SHALL APPLY TACK COAT IN ACCORDANCE WITH FDOT SECTION 300 PRIOR TO PLACEMENT OF OVERLAY.
- MILLING OPERATION SHALL COMPLY WITH FDOT STANDARD SPECIFICATIONS FOR ROADWAY CONSTRUCTION, SECTION 327.
- EXISTING ASPHALT PAVEMENT SHALL BE A MINIMUM OF 1.5" TYPE S3 (SP9.5) ASPHALT SURFACE COURSE IN ACCORDANCE WITH FDOT STANDARD SPECIFICATIONS SECTION 331.
- CLEAN EXISTING ASPHALT PAVEMENT OF ALL LOOSE MATERIAL, GRAVEL, AND FOREIGN MATTER BY BROOMING AND VACUUMING PRIOR TO APPLICATION OF TACK COAT OR ASPHALT OVERLAY.
- CONTRACTOR SHALL CLEAN THE CONSTRUCTION AREA OF ALL DEBRIS, AGGREGATES, ETC. ALL WASTE MATERIAL SHALL BE REMOVED FROM SITE AND DISPOSED OF IN A LEGAL MANNER.
- FINAL STRIPPING TO BE TWO COATS OF LATEX BASE TRAFFIC PAINT.
- CONTRACTOR SHALL RECORD CONFIGURATION OF EXISTING STRIPPING WITHIN AREAS TO RECEIVE OVERLAY, AND BE RESPONSIBLE FOR RESTRIPPING THESE AREAS.
- REPLACE ANY DAMAGED WHEEL STOPS AND CURBING WITHIN AREAS OF WORK.
- CONTRACTOR SHALL ENSURE POSITIVE DRAINAGE WHERE PROPOSED ASPHALT OVERLAY ABUTS EXISTING ASPHALT BY MILLING AND/OR FEATHERING TO MINIMIZE BIRD BATHS.
- ENSURE POSITIVE DRAINAGE BY MATCHING EXISTING DRAINAGE PATTERN WHERE NECESSARY DUE TO EXISTING BIRD BATHS/DAMAGED PAVEMENT. CONTRACTOR SHALL REPAIR THESE AREAS PRIOR TO OVERLAY TO CORRECT ANY DRAINAGE ISSUES.
- CONTRACTOR HAS THE OPTION OF REMOVING EXISTING ASPHALT PAVEMENT BY MECHANICAL MEANS IN ITS ENTIRETY, HOWEVER ANY COSTS ASSOCIATED WITH REMOVAL OF ENTIRE SURFACE COURSE, REPAIR OF DAMAGED BASE, AND NECESSARY WORK TO MATCH EXISTING GRADES AT SITE IMPROVEMENTS SHALL BE THE RESPONSIBILITY OF CONTRACTOR.
- ANY EXPOSED BASE COURSE DURING MILLING AND/OR ASPHALT REMOVAL OPERATION SHALL BE PRIME COATED WITHIN THE SAME DAY. AVOID EXPOSURE OF BASE COURSE TO THE ELEMENTS.

ADDITIONAL NOTES/SEPCS

- PERMITS: THE CONTRACTOR SHALL OBTAIN THE NECESSARY PERMITS (ORANGE COUNTY, "RIGHT-OF-WAY UTILIZATION PERMIT", AND OTHER APPLICABLE AUTHORIZATION), PRIOR TO ANY ROADWAY WORK. ADDITIONALLY, THE CONTRACTOR SHALL PROVIDE ADVANCE NOTICE TO THE APPROPRIATE AUTHORITY, AS REQUIRED, PRIOR TO CONSTRUCTION OPERATIONS.
- PROTECTION OF EXISTING IMPROVEMENTS: THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL PAVEMENTS, AND OTHER IMPROVEMENTS WITHIN THE WORK AREA. ALL DAMAGE TO SUCH IMPROVEMENTS, AS A RESULT OF THE CONTRACTOR'S OPERATIONS, BEYOND THE LIMITS OF THE WORK OF PAVEMENT REPLACEMENT AS DESCRIBED HEREIN, SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- STREET OR ROADWAY PAVEMENT CUT AND REMOVED IN CONNECTION WITH TRENCH EXCAVATION SHALL BE REPLACED OR RESTORED IN EQUAL OR BETTER CONDITION THAN THE ORIGINAL AND AS SHOWN ON THE DRAWINGS. THE DRAWINGS INDICATE MINIMUM REQUIREMENTS.
- MATERIALS, INCLUDING SOIL CEMENT, BITUMINOUS PRIME AND TACK COAT, AND ASPHALTIC CONCRETE FOR THE ABOVE WORK SHALL MEET THE REQUIREMENTS ESTABLISHED THEREFORE BY THE FDOT SPECIFICATIONS.
- PAVEMENT REMOVAL:
 - WHERE EXISTING PAVEMENT IS TO BE REMOVED, THE SURFACING SHALL BE MECHANICAL SAW CUT PRIOR TO REMOVAL, MILLING AND TRENCH EXCAVATION, LEAVING A UNIFORM AND STRAIGHT EDGE, WITH MINIMUM DISTURBANCE TO THE REMAINING ADJACENT SURFACING. IMMEDIATELY FOLLOWING THE SPECIFIED BACKFILLING AND COMPACTION, A TEMPORARY SAND SEAL COAT SURFACE SHALL BE APPLIED TO THE CUT AREAS. THIS TEMPORARY SURFACING SHALL PROVIDE A SMOOTH TRAFFIC SURFACE WITH THE EXISTING ROADWAY AND SHALL BE MAINTAINED UNTIL FINAL RESTORATION, FOLLOWING THE ABOVE OPERATION, THE CONTRACTOR SHALL PROCEED IMMEDIATELY WITH FINAL PAVEMENT RESTORATION IN ACCORDANCE WITH THE REQUIREMENTS SET FORTH IN THE SEMINOLE COUNTY, "RIGHT-OF-WAY UTILIZATION REGULATIONS", AND THESE STANDARDS.
 - SOIL CEMENT BASE COURSE SHALL BE COMPACTED FOR ITS FULL THICKNESS TO NOT LESS THAN 95 PERCENT OF MAXIMUM DENSITY AS DETERMINED BY AASHTO DESIGNATION T-180. FIELD DENSITY OF SOIL CEMENT BASE IN PLACE SHALL BE DETERMINED BY AASHTO DESIGNATION T-191 OR ASTM DESIGNATION D2922.
 - CONSTRUCTION METHODS AND EQUIPMENT SHALL GENERALLY MEET THE REQUIREMENTS THEREFORE AS ESTABLISHED IN THE FDOT SPECIFICATIONS, BUT SHALL BE MODIFIED TO MEET THE RELATIVELY NARROW STRIP CONSTRUCTION CONDITIONS. ANY SUCH MODIFICATIONS SHALL BE APPROVED BY THE ENGINEER PRIOR TO THEIR USE.
 - AFTER THE APPLICATION OF THE PRIME COAT ON THE BASE, THE PRIME COAT SHALL BE ALLOWED TO CURE WITHOUT SANDING FOR A PERIOD OF 24-HOURS. THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO PROTECT THE PRIMED SURFACE AGAINST DAMAGE DURING THIS INTERVAL. IF, AT THE END OF 24-HOURS, IT IS NOT PROPOSED TO PROCEED AT ONCE WITH THE APPLICATION OF THE SURFACE COURSE, PRIMED SURFACE SHALL BE GIVEN A LIGHT APPLICATION OF CLEAN SAND AND OPENED TO TRAFFIC.
 - JOINTS WITH EXISTING SURFACE AND BASE SHALL BE STRAIGHT AND NEAT. IF NECESSARY TO OBTAIN A STRAIGHT NET JOINT, THE CONTRACTOR SHALL CUT OUT SUFFICIENT EXISTING MATERIAL AND REPLACE IT WITH NEW MATERIAL.
- PAVEMENT RESTORATION - ASPHALT:
 - SOIL CEMENT BASE COURSE SHALL BE COMPACTED FOR ITS FULL THICKNESS TO NOT LESS THAN 95 PERCENT OF MAXIMUM DENSITY AS DETERMINED BY AASHTO DESIGNATION T-180. FIELD DENSITY OF SOIL CEMENT BASE IN PLACE SHALL BE DETERMINED BY AASHTO DESIGNATION T-191 OR ASTM DESIGNATION D2922.
 - CONSTRUCTION METHODS AND EQUIPMENT SHALL GENERALLY MEET THE REQUIREMENTS THEREFORE AS ESTABLISHED IN THE FDOT SPECIFICATIONS, BUT SHALL BE MODIFIED TO MEET THE RELATIVELY NARROW STRIP CONSTRUCTION CONDITIONS. ANY SUCH MODIFICATIONS SHALL BE APPROVED BY THE ENGINEER PRIOR TO THEIR USE.
 - AFTER THE APPLICATION OF THE PRIME COAT ON THE BASE, THE PRIME COAT SHALL BE ALLOWED TO CURE WITHOUT SANDING FOR A PERIOD OF 24-HOURS. THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO PROTECT THE PRIMED SURFACE AGAINST DAMAGE DURING THIS INTERVAL. IF, AT THE END OF 24-HOURS, IT IS NOT PROPOSED TO PROCEED AT ONCE WITH THE APPLICATION OF THE SURFACE COURSE, PRIMED SURFACE SHALL BE GIVEN A LIGHT APPLICATION OF CLEAN SAND AND OPENED TO TRAFFIC.
 - JOINTS WITH EXISTING SURFACE AND BASE SHALL BE STRAIGHT AND NEAT. IF NECESSARY TO OBTAIN A STRAIGHT NET JOINT, THE CONTRACTOR SHALL CUT OUT SUFFICIENT EXISTING MATERIAL AND REPLACE IT WITH NEW MATERIAL.

OCSO SECTOR IV PARKING LOT DESIGN

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PE41046

PERMIT DOCUMENTS

DATE	SUBMISSION / REVISION	NO.

SITE DEVELOPMENT PLAN

SCALE: 1"=20'

DRAWN BY: JF

CHECK BY: MK

DATE: JANUARY 6, 2017

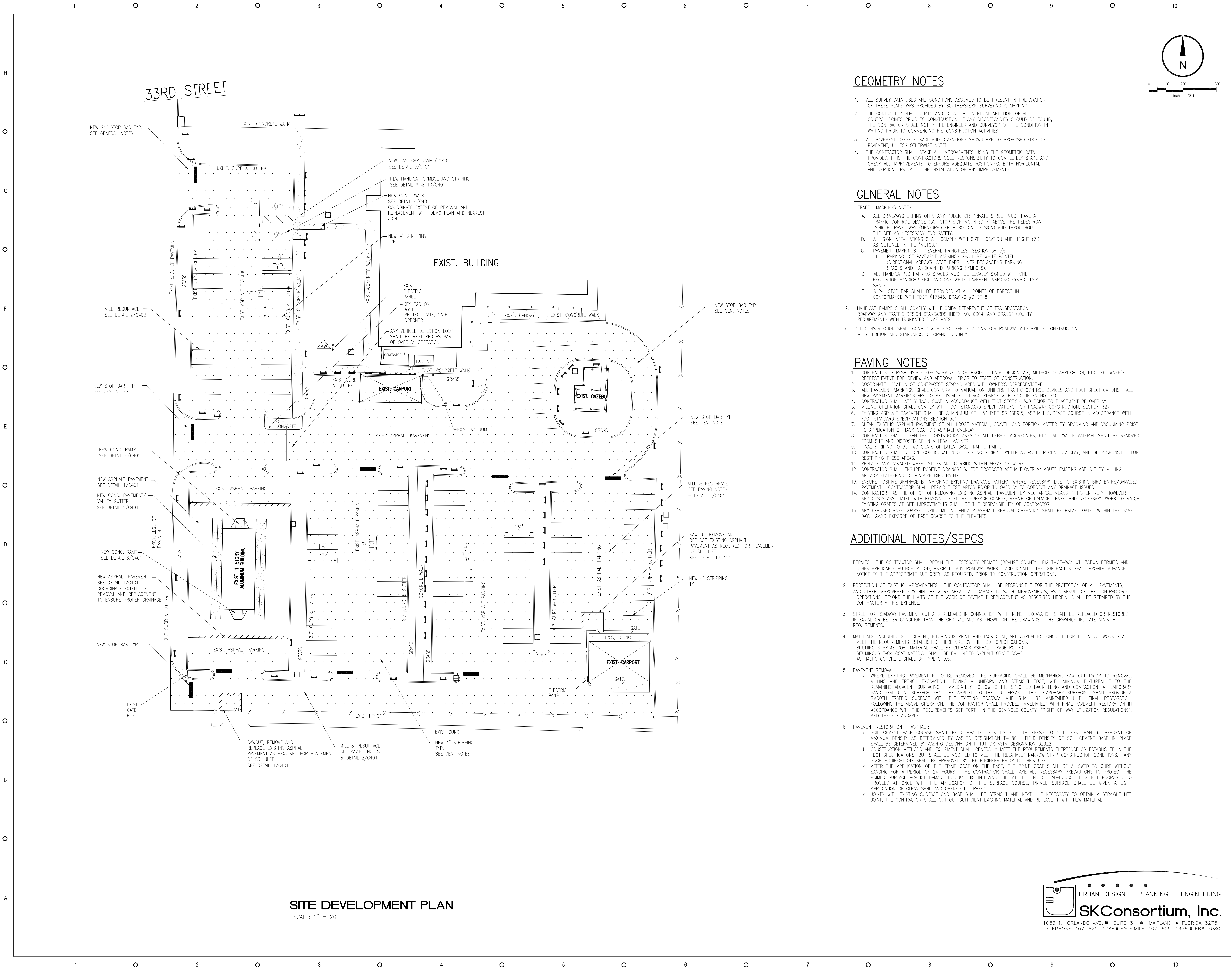
PROJECT NUMBER: 15012-0013

URBAN DESIGN PLANNING ENGINEERING

SK Consortium, Inc.

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TELEPHONE 407-629-4288 ■ FACSIMILE 407-629-1656 ■ E#B 7080

C101



SITE DEVELOPMENT PLAN

SCALE: 1" = 20'

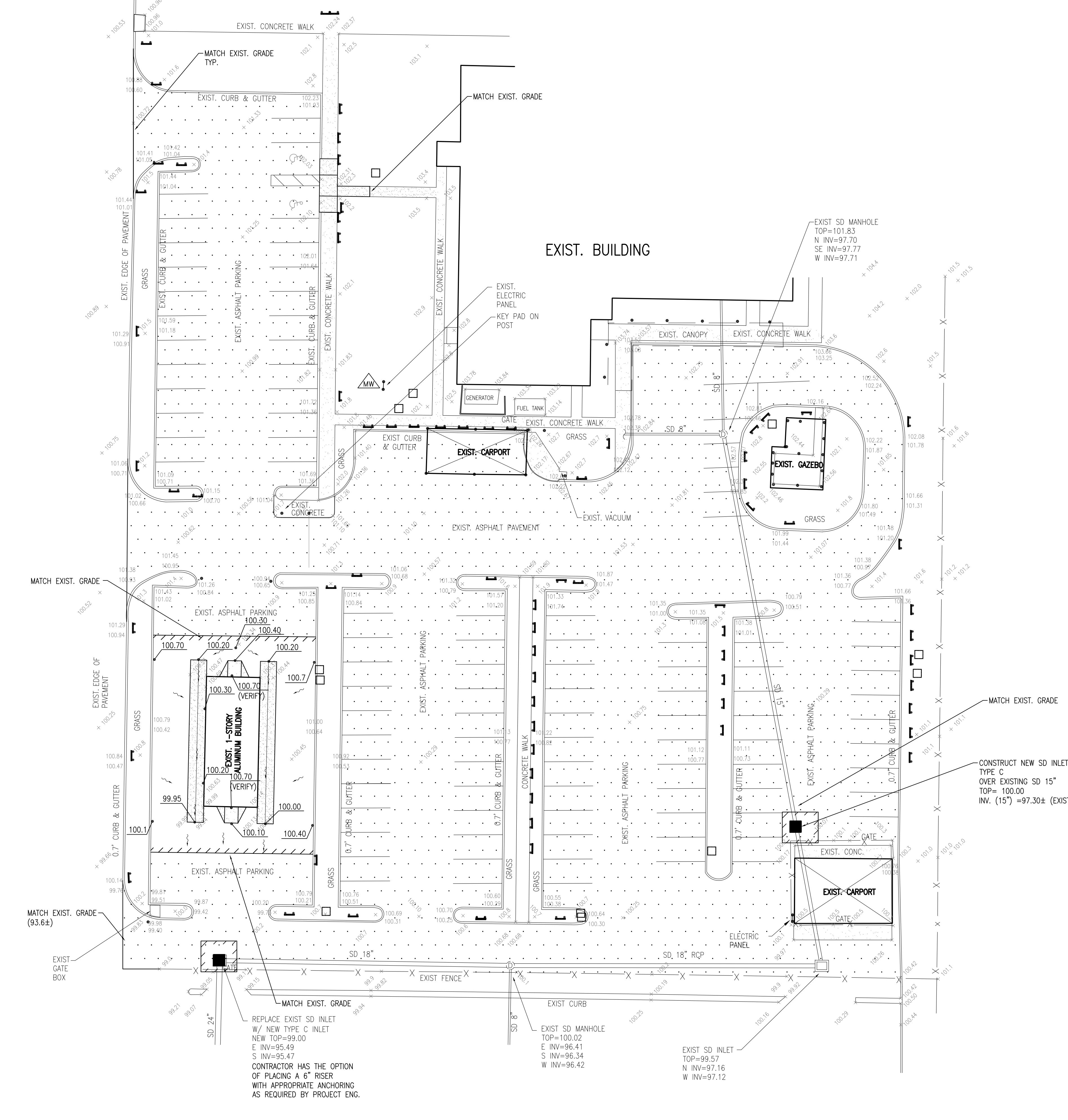
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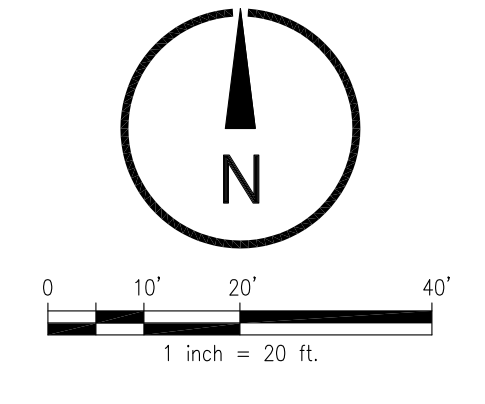
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33RD STREET



GRADING + DRAINAGE PLAN
SCALE: 1" = 20'



GENERAL NOTES:

1. PRIOR TO CONSTRUCTION THE CONTRACTOR SHALL VERIFY LOCATION AND INVERTS OF EXISTING UTILITIES AT PROPOSED CROSSING AND POINTS OF CONNECTION. THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY OF ANY UTILITY CONFLICTS.
2. COMPLY WITH FLORIDA DEPT. OF TRANSPORTATION ROADWAY & TRAFFIC DESIGN STANDARDS ON THE FOLLOWING:
 - 2.A. STORM DRAIN INLET TYPE "C" & "E" : INDEX No. 232 (PROVIDE STEEL ANGLE FOR GRATE SEAT FOR INLETS IN TRAFFIC AREAS)
 - 2.B. TYPE "Y" GUTTER INLET INDEX No. 221
 - 2.C. TYPE "P AND J" MANHOLE INDEX NO. 200
 - 2.D. TYPE "T" INLET INDEX NO. 233
3. REFER TO SHEET C001 FOR EROSION & SEDIMENTATION CONTROL DURING CONSTRUCTION.
4. ALL DISTURBED AREAS SHALL BE SODED. SEE LANDSCAPE PLANS FOR MINIMUM EXTENT OF SODDING ON-SITE.
5. ALL STORM DRAIN INLETS LOCATED IN VEHICULAR TRAFFIC AREAS SHALL RECEIVE A STEEL ANGLE GRATE SEAT. ALL GRATES SHALL BE TRAFFIC RATED. SITE IS NOT LOCATED WITHIN THE 100YR. FLOOD PRONE AREA.

LEGEND

FINISH GRADE	EXIST.	NEW
FLOW PATTERN		
CONTOUR		
STORM DRAIN INLET		
STORM DRAIN LINE		
STORM MANHOLE		

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**OCISO SECTOR IV
PARKING LOT
DESIGN**

2400 W. 33RD ST
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PERMIT DOCUMENTS

DATE	SUBMISSION / REVISION	NO.

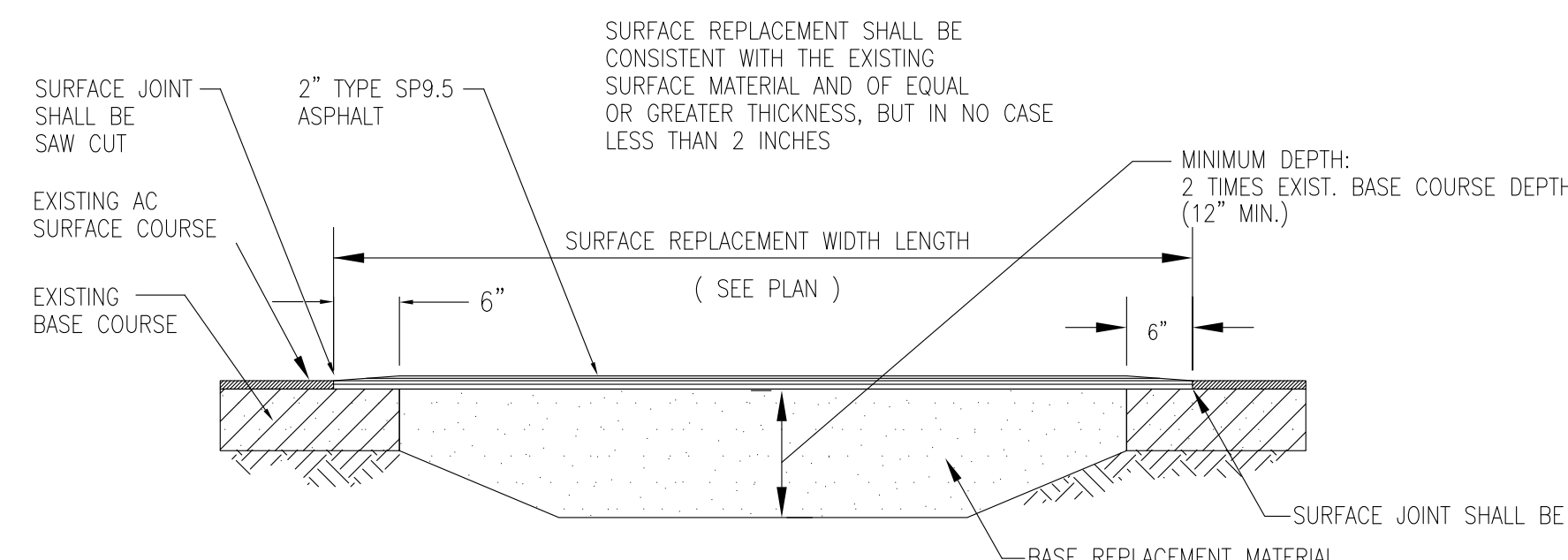
**GRADING + DRAINAGE
PLAN**

SCALE:	1"=20'
DRAWN BY:	JF
CHECK BY:	MK
DATE:	JANUARY 6, 2017
PROJECT NUMBER:	15012-0013

URBAN DESIGN PLANNING ENGINEERING
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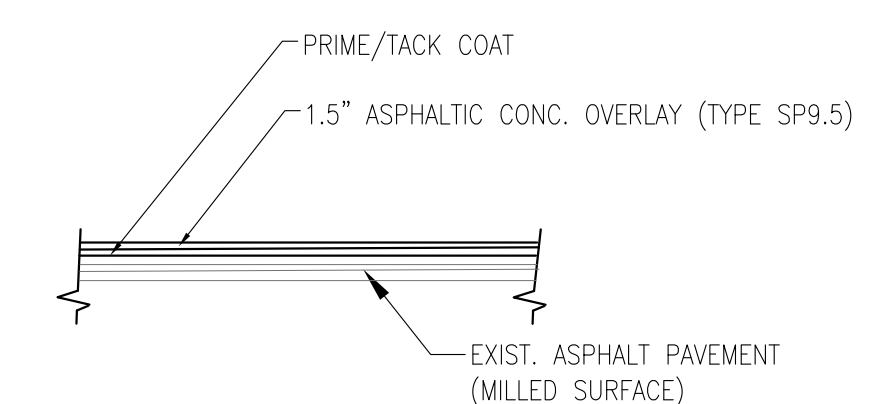
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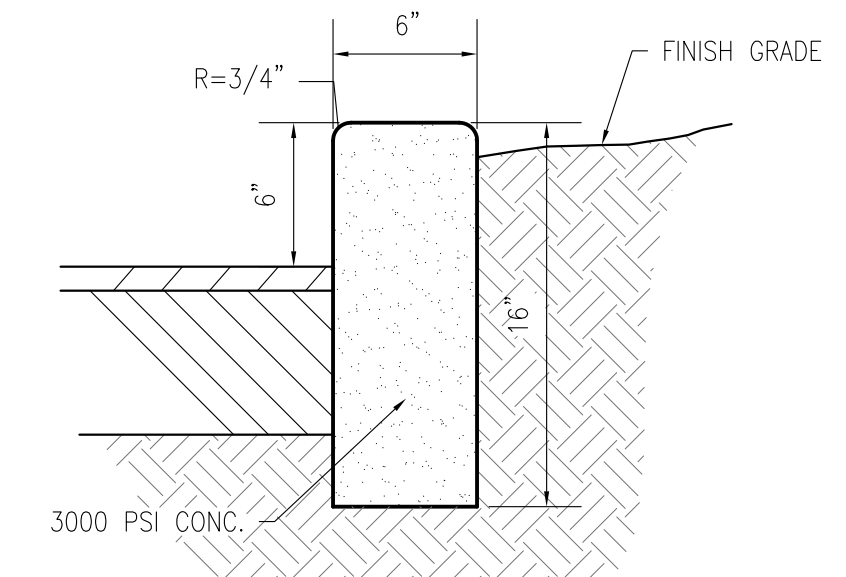


NOTES:
 1.) BACKFILL SHALL BE PLACED IN LAYERS NOT TO EXCEED 6 INCHES, FROM BENEATH THE HAUNCHES OF THE PIPE TO THE BASE AND SHALL BE COMPACTED TO NOT LESS THAN 98% RELATIVE DENSITY IN ACCORDANCE WITH AASHTO T-180.
 2.) IF LIMEROCK IS USED, LBR-100 IS REQUIRED. HOWEVER, IF SOIL CEMENT OR DURAROCK IS USED A 95% DENSITY IS REQUIRED.
 3.) CONTRACTOR SHALL VERIFY LIMITS OF PAVEMENT REMOVAL AND REPLACEMENT IN THE FIELD.

1 PAVEMENT REMOVAL AND REPLACEMENT
 C-401 NOT TO SCALE

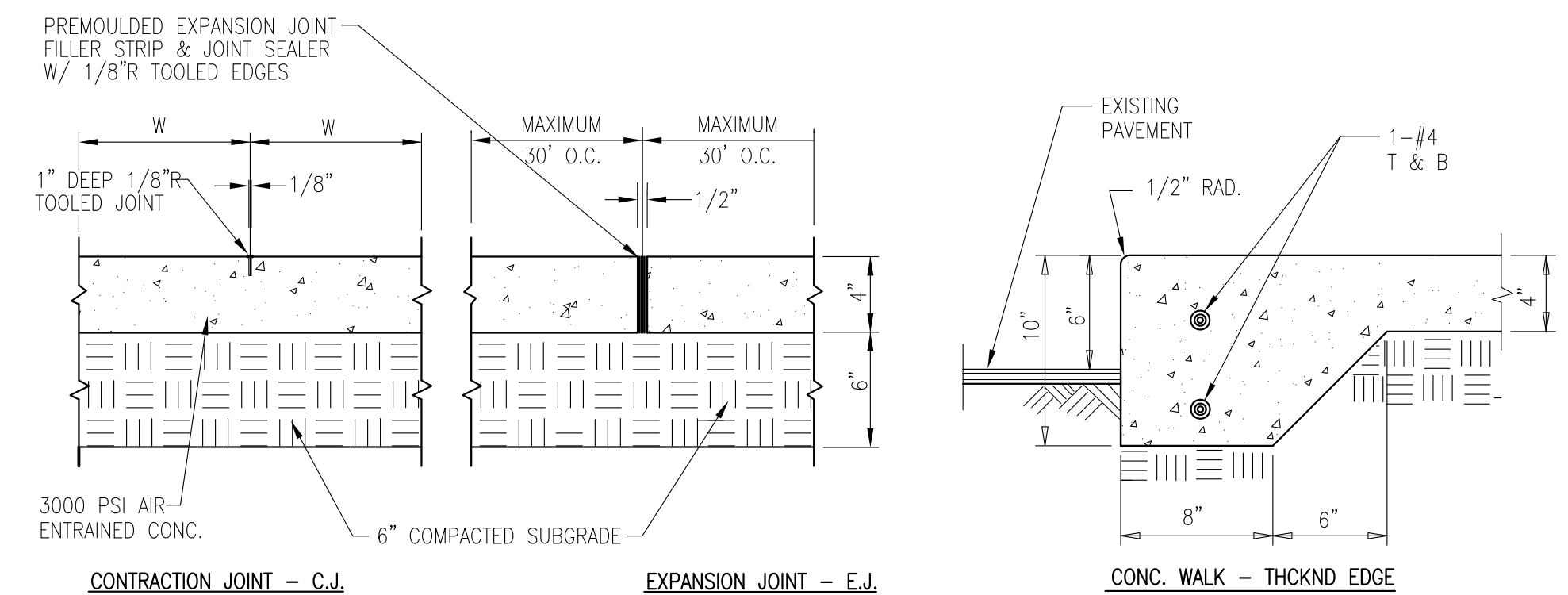


2 ASPHALT PAVEMENT OVERLAY
 C401 1" = 1'-0"



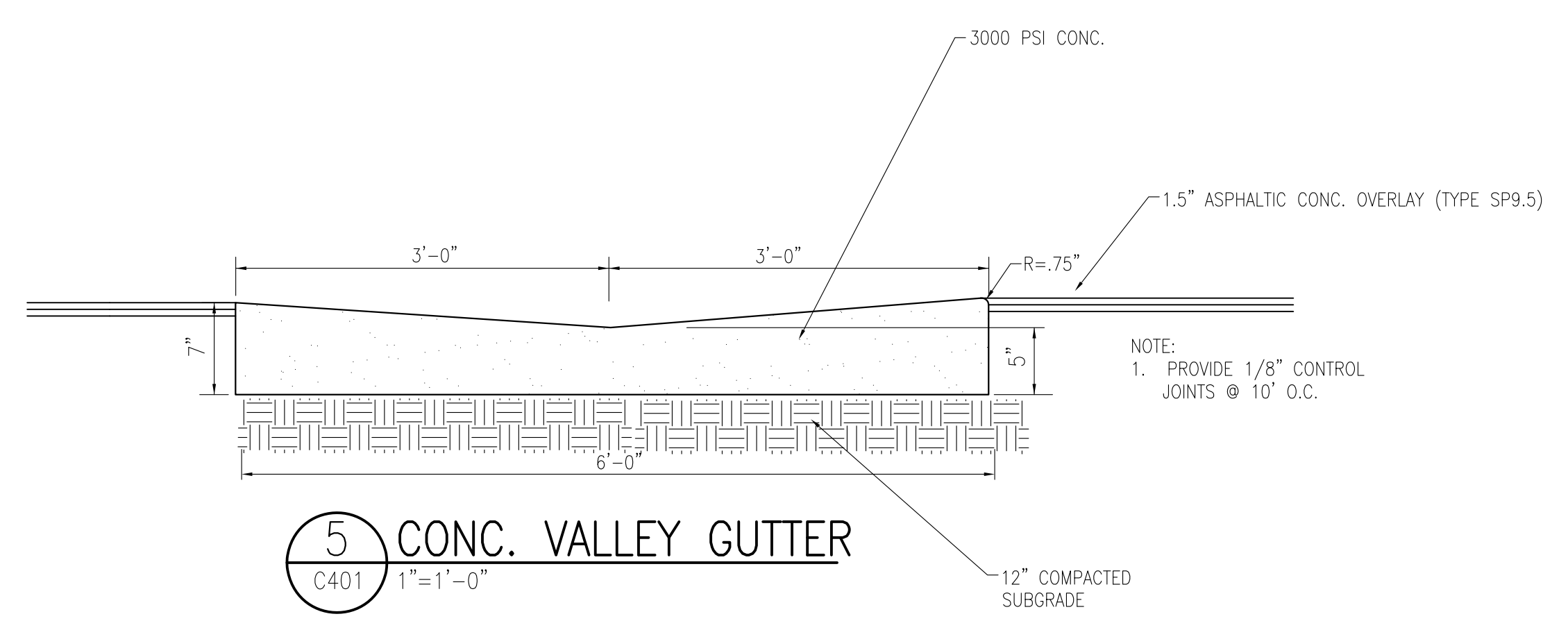
NOTE:
 1. PROVIDE 1/8" CONTROL JOINTS @ 10' O.C.
 2. PROVIDE 6" STABILIZED SUBGRADE BENEATH THE CURB.

3 CONC. CURB
 C401 1-1/2" = 1'-0"

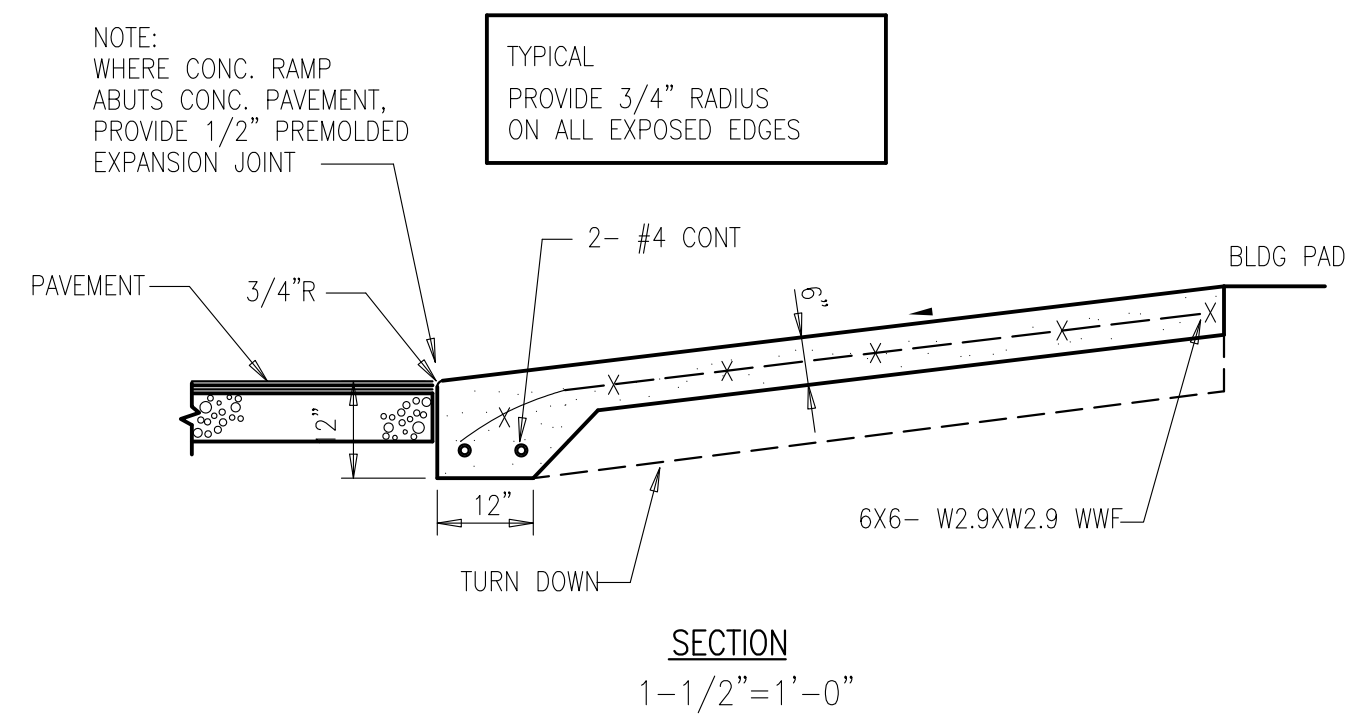


NOTES:
 1. A THICKENED EDGE SHALL BE PROVIDED BETWEEN SIDEWALK AND DRIVEWAYS OR PARKING LOT.
 2. SLOPE CONC. SIDEWALKS AWAY FROM BUILDINGS TO PROVIDE POSITIVE DRAINAGE.
 3. PROVIDE 2% CROSS SLOPE ON CONC. WALKS TYP.
 4. PROVIDE CONTROL JOINTS @ INTERVALS EQUAL TO SIDEWALK WIDTH (W).
 5. PROVIDE PREMOLDED EXPANSION JOINT WHERE CONC. WALK ABUTS BLDG., POLES, AND OTHER CONC. WALKS.

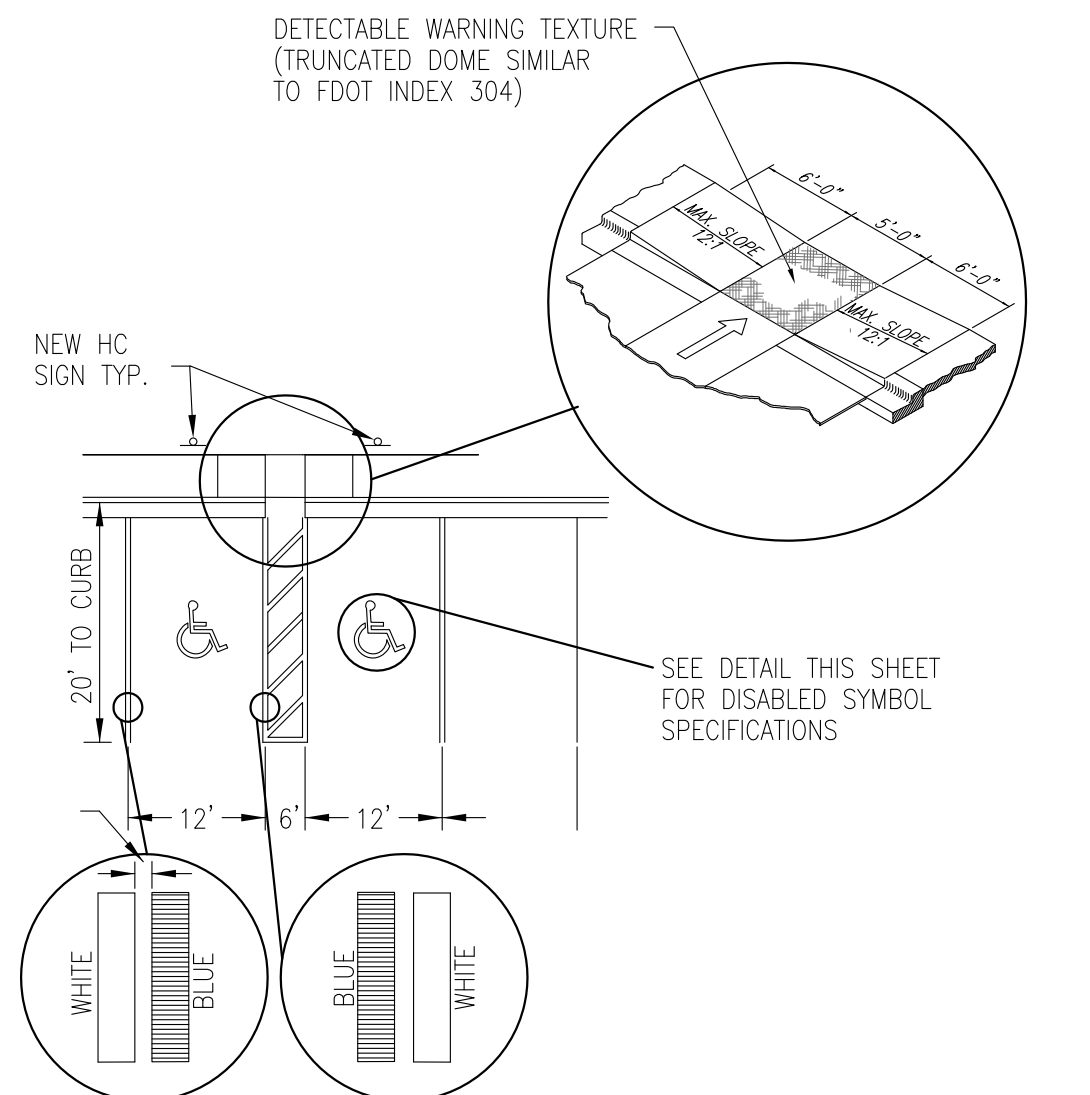
4 CONCRETE SIDEWALK DETAIL
 C401 1-1/2" = 1'-0"



5 CONC. VALLEY GUTTER
 C401 1" = 1'-0"

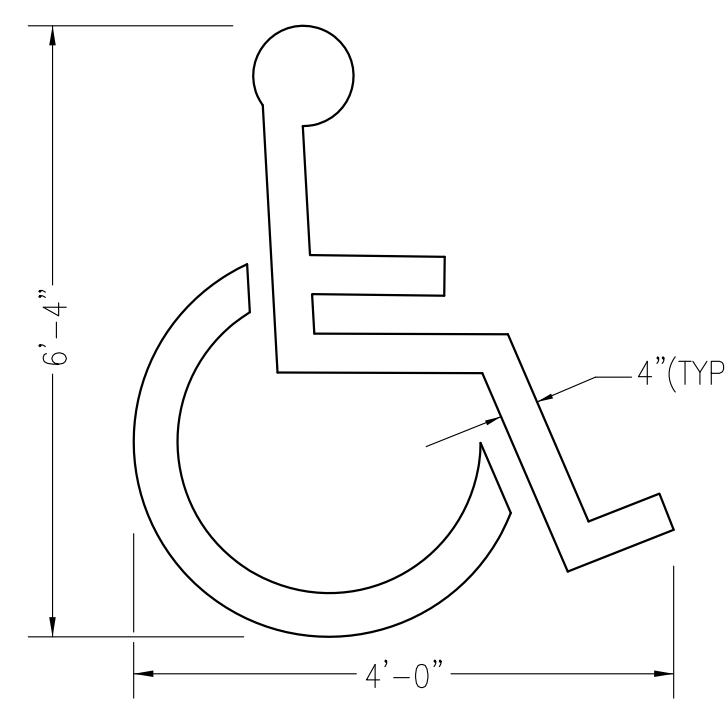


6 RAMP SECTION
 C-401 NOT TO SCALE

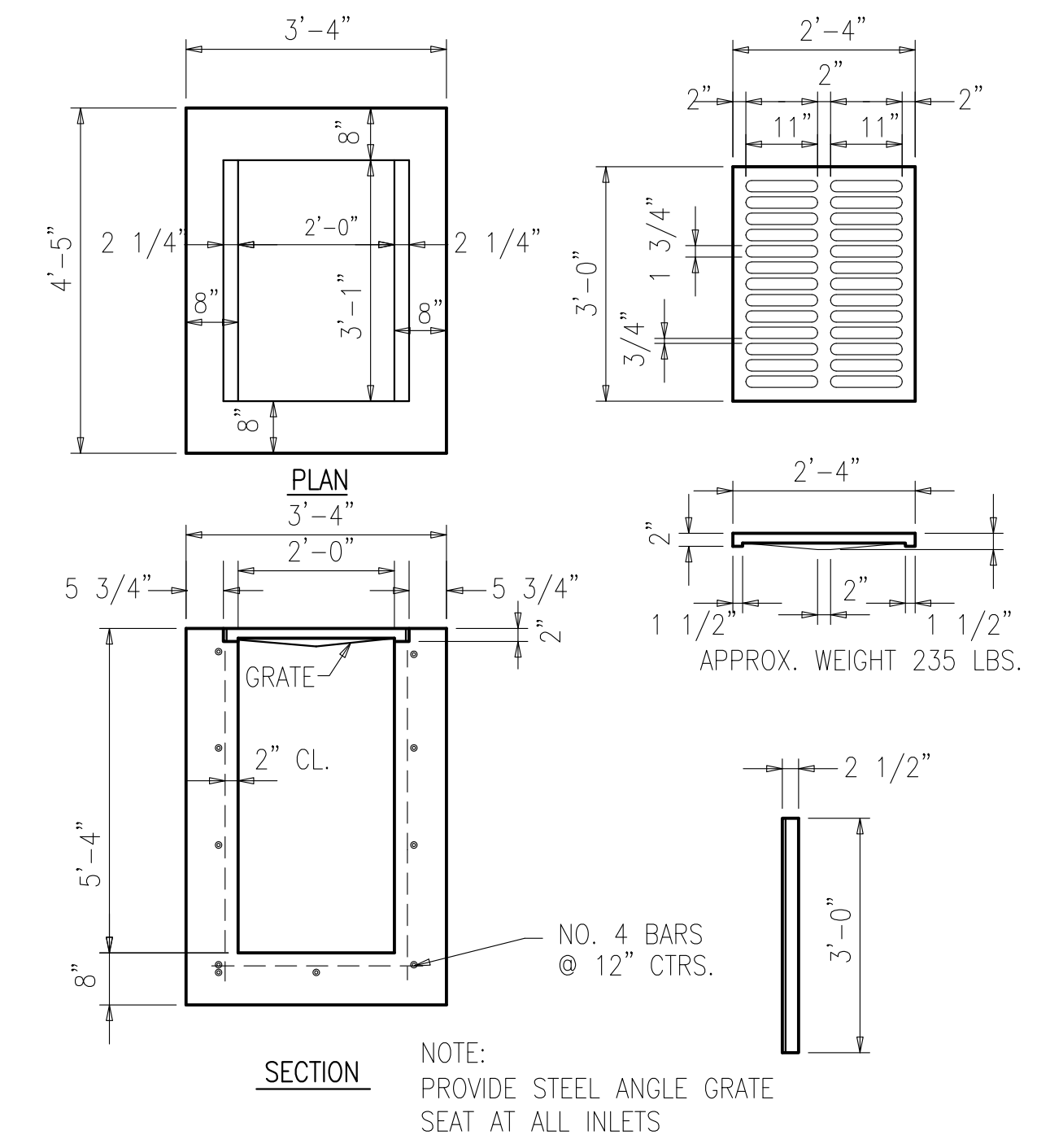


NOTES:
 1. EACH SUCH PARKING SPACE SHALL BE CONSPICUOUSLY OUTLINED IN BLUE PAINT, AND SHALL BE POSTED AND MAINTAINED WITH A PERMANENT ABOVE GRADE SIGN BEARING THE INTERNATIONAL SYMBOL OF ACCESSIBILITY, OR THE CAPTION "PARKING BY DISABLED PERMIT ONLY" OR BEARING BOTH SUCH SYMBOL AND CAPTION. SUCH SIGNS SHALL NOT BE OBSCURED BY A VEHICLE PARKED IN THE SPACE. ALL HANDICAPPED PARKING SPACES MUST BE SIGNED AND MARKED IN ACCORDANCE WITH THE STANDARDS ADOPTED BY THE DEPARTMENT OF TRANSPORTATION.
 2. FL DOT RECOMMENDS MEASURING PARKING SPACE WIDTH FROM CENTER TO CENTER BETWEEN BLUE AND WHITE STRIPES.

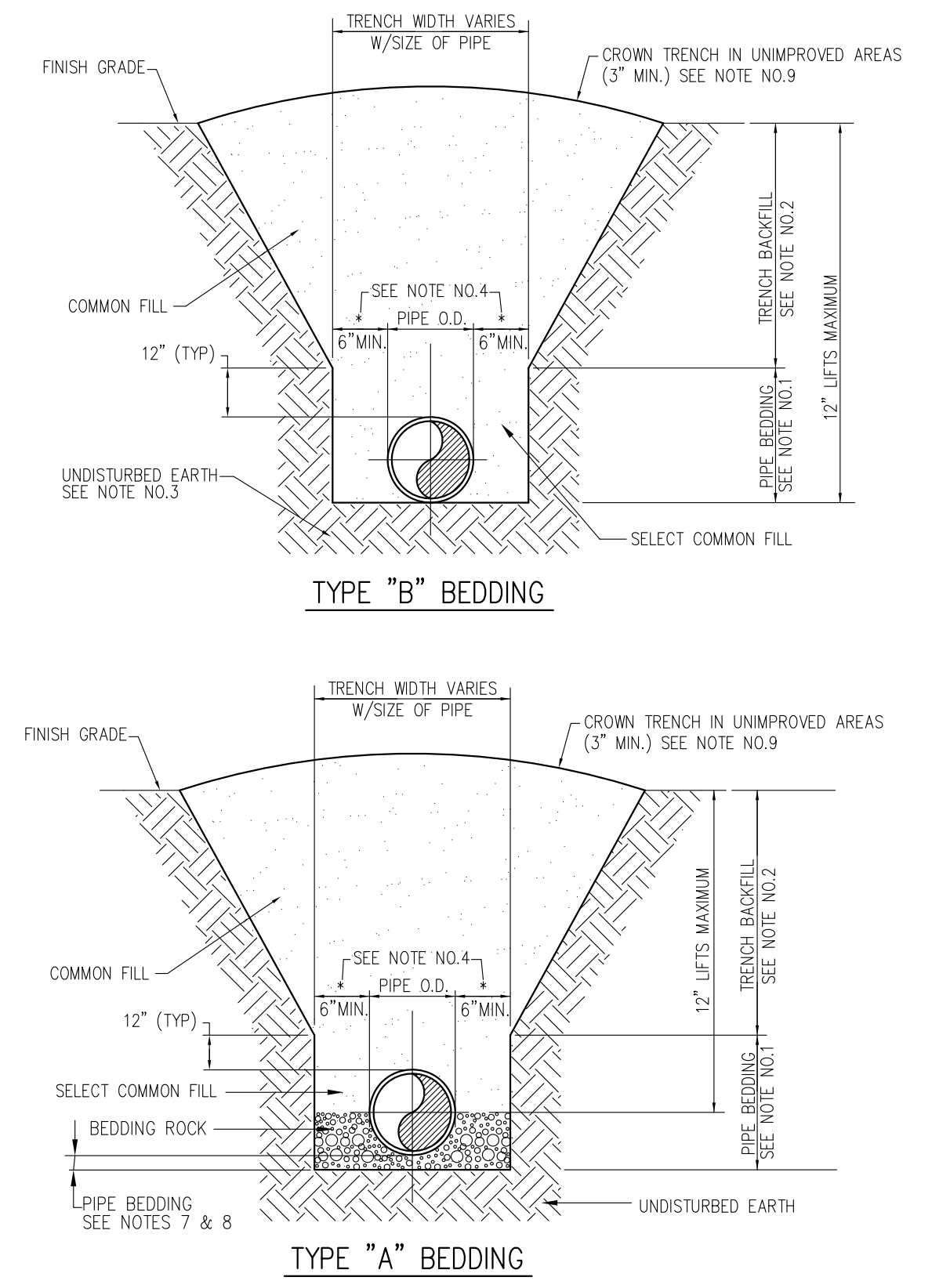
9 HANDICAP STRIPING DETAIL
 C401



10 HANDICAP PARKING SYMBOL
 C401 NOT TO SCALE



7 TYPE "C" INLET DETAIL
 C401 1/2" = 1'-0"



NOTES:
 1. PIPE BEDDING: SELECT COMMON FILL COMPACTED TO 95% OF THE MAXIMUM DENSITY AS PER AASHTO T-180.
 2. TRENCH BACKFILL: COMMON FILL COMPACTED TO 95% OF THE MAXIMUM DENSITY AS PER AASHTO T-180.
 3. PIPE BEDDING UTILIZING SELECT COMMON FILL OR BEDDING ROCK IN ACCORDANCE WITH TYPE A BEDDING TO BE DETERMINED IN THE FIELD AS DIRECTED BY THE COUNTY.
 4. (1) 15" MAX. FOR PIPE DIAMETER LESS THAN 24" AND 24" MAX. FOR PIPE DIAMETER 24" AND LARGER.
 5. WATER SHALL NOT BE PERMITTED IN THE TRENCH DURING CONSTRUCTION.
 6. ALL PIPE TO BE INSTALLED WITH BELL FACING UPSTREAM TO THE DIRECTION OF THE FLOW.
 7. GRAVITY SEWERS SHALL UTILIZE TYPE A BEDDING, AS REQUIRED BY THE COUNTY. BEDDING DEPTH SHALL BE 4" MINIMUM FOR PIPE DIAMETER LESS THAN 15" AND 6" MINIMUM FOR PIPE DIAMETER 15" AND LARGER.
 8. DEPTH FOR REMOVAL OF UNSUITABLE MATERIAL SHALL COVER DEPTH OF BEDDING ROCK BELOW THE PIPE. COUNTY SHALL DETERMINE IN THE FIELD REQUIRED REMOVAL OF UNSUITABLE MATERIAL TO REACH SUITABLE FOUNDATION.
 9. FINAL RESTORATION IN IMPROVED AREAS SHALL BE IN COMPLIANCE WITH ALL APPLICABLE REGULATIONS OF GOVERNING AGENCIES. SURFACE RESTORATION WITHIN THE RIGHT-OF-WAY SHALL COMPLY WITH REQUIREMENTS OF RIGHT-OF-WAY UTILIZATION REGULATIONS AND ROAD CONSTRUCTION SPECIFICATIONS.

8 PIPE BEDDING
 C401 NOT TO SCALE

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OCSO SECTOR IV PARKING LOT DESIGN

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MAJID KALAGHCHI
 PE41046

PERMIT DOCUMENTS

DATE	SUBMISSION / REVISION	NO.

SITE DETAILS

SCALE: 1" = 20'
 DRAWN BY: JF
 CHECK BY: MK
 DATE: JANUARY 6, 2017
 PROJECT NUMBER: 15012-0013

SK Consortium, Inc.
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C401